

DECK REMOVAL NOTES:

DEMOLISH DECK STARTING AT PIER 3, WITH ONE DEMOLITION OPERATION PROCEEDING TOWARD EACH ABUTMENT.

FALL PROTECTION IS PROVIDED BY SUBDECKING AT BAYS AND OVERHANGS. IF SUBDECKING IS NOT CONTINUOUS, PROVIDE CONCRETE DEADMEN AND CABLE HORIZONTAL LIFELINES FOR WORKERS WHERE FALL HAZARD EXISTS. PROVIDE A CONTROL LINE AT ABUTMENTS, AND TEMPORARY HANDRAILS AS REQUIRED AT WINGWALLS.

INSTALL PLASTIC SHEETING INSIDE SUBDECKING OVER ROAD, SIDEWALK, AND STREAM, TO CONTAIN SAWCUT SLURRY.

BEFORE SAWCUTTING DECK, DEMOLISH A SMALL SECTION TO DETERMINE CONNECTOR LOCATIONS. LAY OUT SAWCUTS SO AT LEAST OF 2" SLAB WIDTH BEARS ON THE GIRDER FLANGE (TYP LAYOUT SHOULD BE ABOUT 3" TO BREAK FREE OF SHEAR SPIRALS).

DEMOLITION EQUIPMENT ON DECK DURING DECK REMOVAL (2 SETUPS NEEDED):
 SAWCUT SUBCONTRACTOR TO PROVIDE SAWS AND RELATED EQUIPMENT
 CAT 324 OR 329 EXCAVATOR, WITH SCRAPER FOR PAVEMENT, HOE RAM, AND SLAB CRAB
 SEMI TRACTOR WITH 48' FLATBED TRAILER
 10-WHEEL DUMPTRUCK
 CAT 420 BACKHOE-LOADER WITH HOE-RAM

GENERAL SEQUENCE FOR DECK REMOVAL, PER SPAN:

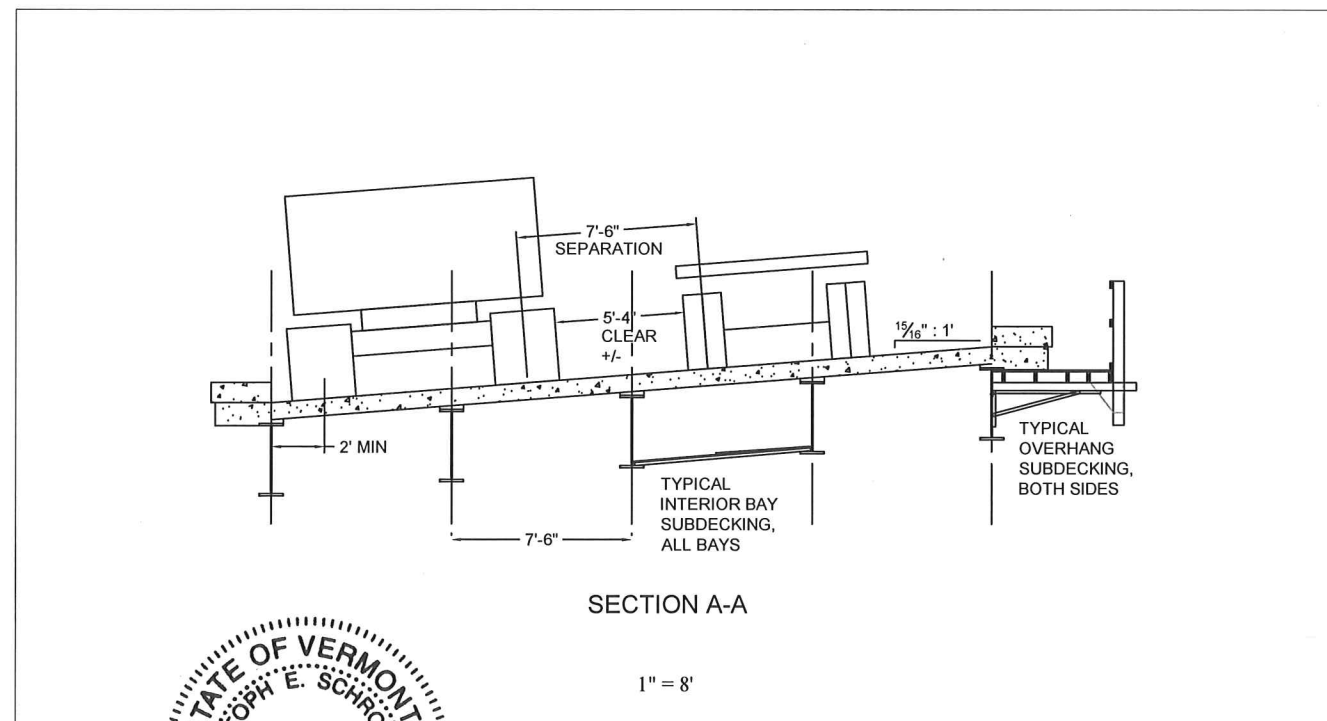
- 1) REMOVE PAVEMENT AND MEMBRANE
- 2) TRANSVERSE SAWCUTS
- 3) CHIP CONCRETE ON OVERHANGS AS NEEDED TO COMPLETE TRANSVERSE CUTS
- 4) INTERIOR LONGITUDINAL CUTS
- 5) LONGITUDINAL CUTS ALONG EXTERIOR GIRDERS, REMOVING OVERHANG SECTIONS AS THEY ARE CUT FREE (SEE DETAIL).
 OPTIONALLY, MAY LEAVE CUTOFF SECTIONS RESTING ON OVERHANG SUBDECKING UNTIL REMOVAL.
- 6) CHIP OUT DECK NEAR DIAPHRAGMS TO ALLOW ACCE FOR EXCAVATOR AND TO INSTALL X-BRACES.
- 7) INSTALL TEMPORARY X-BRACING AT START OF WORKING SPAN (SEE DETAIL)
- 8) DEMOLISH CONCRETE DIAPHRAGM AT START OF SPAN, AND END OF PREVIOUS SPAN, BY SAWCUTTING AND CHIPPING
- 9) REMOVE INTERIOR SAWCUT PANELS
- 10) USE 420 WITH HOE-RAM TO REMOVE MUCH OF THE CONCRETE REMAINING ON TOP FLANGES OF GIRDERS.
- 11) CHIP CONCRETE REMAINING ON FLANGES ONTO SUBDECKING, AND CUT OFF SHEAR CONNECTORS.

KEEP EXCAVATOR TRACKS IN FULL CONTACT WITH THE DECK.

EXCAVATOR AND TRACTOR-TRAILER ARE TO PULL UP ALONGSIDE EACH OTHER, WITH SEPARATION AS SHOW IN SECTION. DUMPTRUCK IS TO BE LOCATED ON SEPARATE SPAN FROM EXCAVATOR WHEN LOCATED IN-LINE WITH ESCAVATOR.

LIMIT TRAVEL SPEEDS ON CUT DECK TO 5 MPH.

TRAILER MAX LOAD IS 60,000 LB EVENLY DISTRIBUTED (THIS IS 80 BAY-FEET OF SLAB AT 750 LB/FT, +/-).
 DUMPTRUCK GVWR IS 64,000 LB.

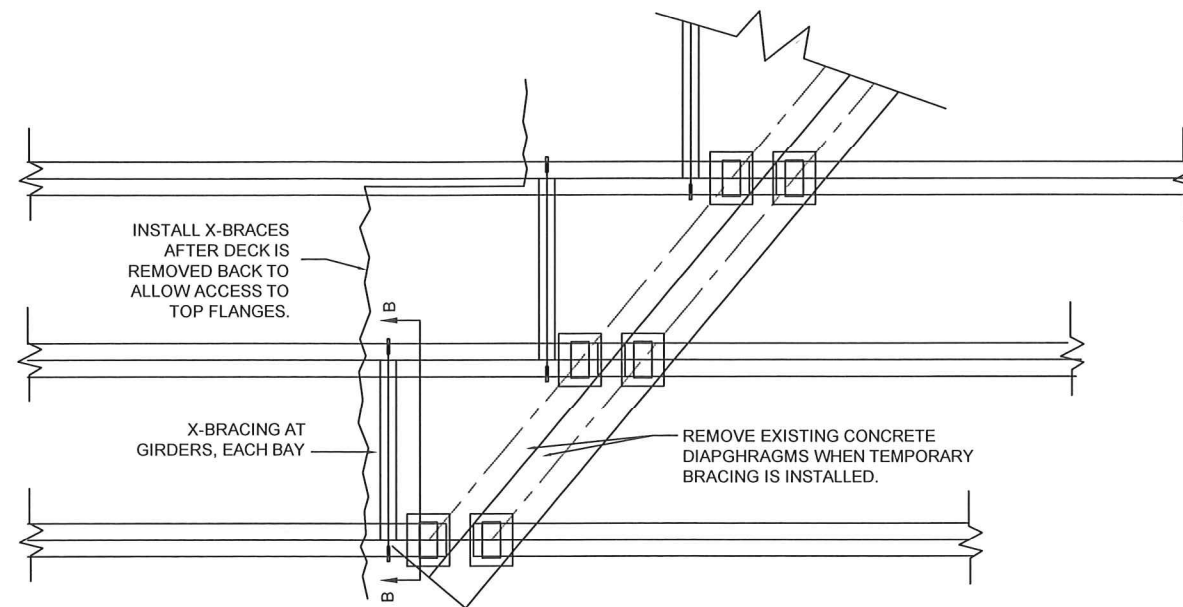


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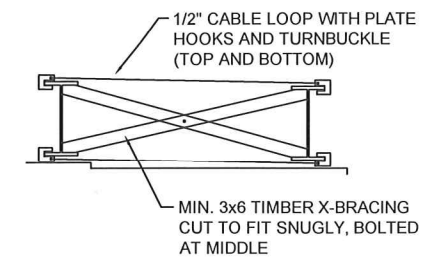
SHEET 1 OF 2

REV: 2-19-15
 DUMPTRUCK AND
 BACKHOE

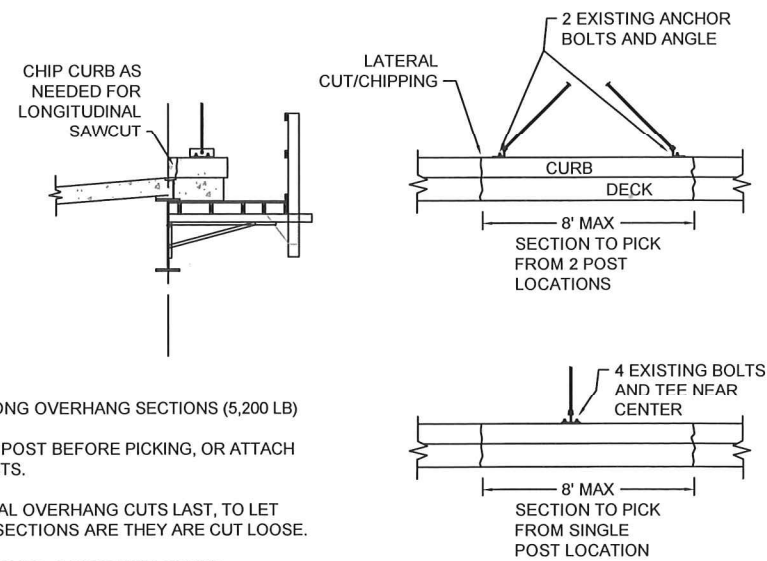
TITLE: DEMOLITION PLAN - SEQUENCE	
PROJECT:	WATERBURY IM 089-2 (43)
SCALE:	1" = 16'
DATE:	FEB. 13, 2015
BECK & BELLUCCI, INC. FRANKLIN, NEW HAMPSHIRE	



TEMPORARY DIAPHRAGMS FOR DECK REMOVAL



LATERAL BRACING DETAIL SEC. B-B, SEPARATE X-BRACING IN EACH BAY



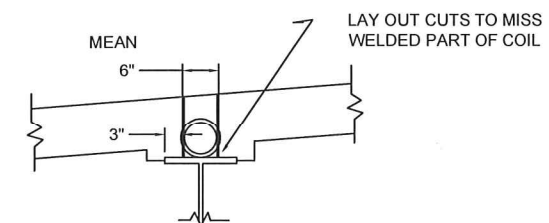
REMOVE MAX. 8' LONG OVERHANG SECTIONS (5,200 LB)

REMOVE RAIL AND POST BEFORE PICKING, OR ATTACH STRAPS ONTO POSTS.

MAKE LONGITUDINAL OVERHANG CUTS LAST, TO LET EXCAVATOR PICK SECTIONS ARE THEY ARE CUT LOOSE.

IF POSTS ARE REMOVED, BOLT DOWN LIFTING HARDWARE TO MIN. 4 BOLTS (2x2 POSTS OR 4x1 POST)

OVERHANG SECTION REMOVAL



LONGITUDINAL SAWCUT LAYOUT (NTS)

